

5. *Design Guidelines for Development on Private Properties*

The Design Guidelines for Development on Private Properties are intended to be a guide for everyone involved in new development and remodeling projects that occur within the East 14th Street South Area. Developers, property owners, and architects should use the guidelines and refer to related San Leandro Zoning Code requirements early in the design process to avoid redesigning portions of the project due to incompatibility with regulations set forth in these documents. Developers are encouraged to meet early with City staff.

Application of the design guidelines and zoning regulations will help shape the look of new development on private properties throughout the corridor. It will, over time, also advance the consistent presence of design elements that create an environment supportive of pedestrian activities. The design guidelines were written with the intent that private development should enhance the existing built environment and complement anticipated public streetscape improvements for the corridor (see Chapter 6).

The guidelines address a variety of development characteristics such as Building Height and Setbacks (Massing), Façade Design, Landscaping, Parking (including access to parking), and Building Design (including materials, awnings, roof lines, lighting etc.). Where applicable, the guidelines also provide specific reference to particular corridor districts and to conditions applicable to properties fronting onto more than one street.

5.1 Introduction

This section discusses the applicability of the Design Guidelines, and their relevance with respect to desired changes in district character.

5.1.1 Applicability of Guidelines and Relationship to Zoning Ordinance

The following paragraphs address the relationship between the San Leandro Zoning Code and the Design Guidelines, define the area of applicability and which projects will be required to follow the guidelines. Consideration is also given to the outreach process that informs the public of new proposals.

Relationship to Zoning Ordinance

The guidelines presented in this chapter of the East 14th Street South Area Development Strategy will be closely linked to the San Leandro Zoning Code. The guidelines and zoning regulations will provide additional design parameters in order to implement the policies of the Development Strategy.

Zoning Ordinance and guidelines will be linked through the creation of three new East 14th Street South Area (SA) Zoning Districts SA-1, SA-2, and SA-3. The regulations for these new Zoning Districts will identify preferred types of land uses in each district and also establish development limits with respect to building heights, setbacks, floor area ratio (FAR), new parking standards, and other relevant regulations specific to the South Area districts (Please refer to *Appendix 3: East 14th Street South Area Zoning Matrix*).

The new zoning districts are as follows:

- **SA-1 – South Area 1**

The SA-1 zoning would be applied to two of the five Districts within the East 14th Street South Area including the **Palma District and the International & Cultural District**. The intent of these zoning regulations would be to promote quality mixed-use developments, especially multi-story developments, which serve the neighborhood and address vehicular traffic issues.

The two Districts to which the SA-1 zoning would apply currently include the most activity in terms of existing retail shopping, and include several opportunity sites that could accommodate new development.

- **SA-2 – South Area 2**

The SA-2 zoning would be applied only to the **McKinley Residential District** and would promote opportunities for new infill residential, including multi-family residential uses that would be sensitive to the existing neighborhoods adjacent to properties fronting the East 14th Street Corridor. A mixture of residential, commercial and community-oriented uses would also be encouraged in multi-story buildings.

The McKinley Residential District currently contains the highest number of residential uses along the East 14th Street Corridor.

- **SA-3 South Area 3**

The SA-3 zoning would be applicable only to the **Gateway District** at the southern end of the East 14th Street South Area. The purpose of these zoning regulations is to provide opportunities for larger office developments, up to 5 stories in height, and to promote additional commercial opportunities that would employ quality design. The Gateway District is envisioned to accommodate landmark buildings that will beautify and enhance southern entryway into San Leandro.

- **CD Commercial Downtown**

In conjunction with City Council direction for Downtown, development, the South Area subdistrict, **Southern Down**, was rezoned to CD- commercial Downtown in February of 2004.

Unless otherwise stated in the San Leandro Zoning Code, all generally applicable requirements shall also apply to properties and projects in the South Area.

Applicability of Design Guidelines

The Design Guidelines will apply to all properties located within one of the three South Area Zoning Districts of the East 14th Street South Area. The boundaries of applicability are indicated in Figure 5.1.

Within these boundaries, the Design Guidelines will be applicable to:

1. Any project within the South Area that requires Site Plan Review that may be required per the San Leandro Zoning Code, and
2. All remodeling projects and changes in use that require city-issued permits.

Some of the properties within the East 14th Street South Area have frontages not only along East 14th Street but also along side streets and, in some cases, streets that run parallel to East 14th Street. This document therefore includes several guidelines that distinguish between Primary and Secondary Frontage (see Figure 5.1), and by addressing how buildings on transition frontages should relate to existing development on adjoining properties.

Approval Process for New Development in the Corridor

It is anticipated that the existing requirements contained in the San Leandro Zoning Code for Site Plan Review will be applied to all development proposals. City staff may encourage certain projects to provide additional community outreach in order to better inform the public of proposed development projects. City staff should be consulted early in the review process. Specific subjects that should be given consideration during such outreach efforts include:

1. Accommodation and amount of required off-street parking;
2. Hours of operation of various businesses in mixed-use projects;
3. Possible shadowing impacts on adjacent buildings and homes located in adjacent residential neighborhoods; and
4. Aesthetic compatibility of a project with goals expressed in the South Area Development Strategy.

It should be noted that such a community outreach process is not in lieu of any required discretionary review process that may be needed for final approval and should be viewed as an opportunity to build a dialogue with residents and affected businesses in the East 14th Street Corridor for the purposes of achieving community consensus.

5.1.2 Corridor and District Character

Most guidelines in this document apply to all properties within the East 14th Street South Area. However, some guidelines relate to issues specific to one of the five South Area Districts described in Chapter 4. These district-specific guidelines reflect and enhance the physical and land use distinctions between the different Corridor Districts and their differences in land uses.

In addition to observing the district-specific guidelines it will be important to interpret many of the generally applicable guidelines in a 'district-sensitive' way. The following paragraphs therefore provide a discussion of the district character that the guidelines are intended to bring about and or enhance (please also refer to Section 4.1, for further discussion of the East 14th Street Corridor Districts). This discussion provides useful background and guidance for interpretation of the guidelines that apply to all properties.

Figure 5.1

11X17

The photo simulations presented in this section illustrate development options for some of the opportunity sites. The development options were the result of the Illustrative Development Case Study work on prototypical and site-specific opportunity sites in the corridor conducted as part of this project (see Chapter 4 and *Appendix 4: Development Case Studies*). The shown examples were all designed in accordance with the Design Guidelines in this document and therefore illustrate what results can be achieved and are desirable through the application of the guidelines.

Southern Downtown District

The application of the guidelines will support a transition of this district toward **becoming an extension of the Downtown**. New buildings should be oriented parallel to East 14th Street and form a continuous frontage of storefront façades. The architectural definition of first floors of multi-story structures should respect and integrate roof and cornice lines of adjacent single story buildings to provide visual integration in locations where single story buildings prevail. Where properties are assembled to form larger sites, it is important to locate store and residential entries within the façade at a frequency that continues the pattern of 40 to 50-foot wide storefronts in the area to the north.

Auto-oriented uses should, over time, be phased out in this district and high quality retail and mixed-use be encouraged. The scale of uses should conform with the gross targets set forth in the District Land Use Matrix (see Section 4.1). If auto-oriented uses require new permitting (i.e. a change in ownership) consideration should be given to upgrading parking lot frontages per these design guidelines.

Although the Southern Downtown has been rezoned to Commercial Downtown, all discretionary approvals will rely on the Development Strategy including Design Guidelines as well as the new South Area parking standards.

McKinley Residential District

The strong existing presence of residential land uses and the envisioned transformation of this District into a mostly residential district is reflected by **design guidelines that specifically address residential setbacks and landscaped front yards** intended to facilitate the level of privacy required for first floor residential uses. Examples of desirable characteristics for residential developments (with a small mixed use component) are illustrated in two photo simulations of development options on the Bellini Foundation site at East 14th Street/Estabrook Avenue (Figures 5.2 and 5.3). Expanded and landscaped tree wells and the use of portions of the public right-of-way for additional landscaping in front of the Sandpiper development



Figures 5.2 and 5.3: Simulations of development options at the Bellini Site in the McKinley Residential District

and Trailer Heaven would further enhance the District’s residential character. Design guidelines, parking strategies and zoning requirements for this area also specifically address concerns about the integration of parking into residential developments.

Palma District

The Palma District, located between San Leandro Boulevard and 141st Avenue, presents the opportunity for the **development of a new focus for the East 14th Street South Area.**

Except for two one-story retail/restaurant buildings of architectural merit, located at the two opposite corners of 135th Avenue, no buildings and façade treatments exist that could credibly give guidance to the future development within the Palma District. It will therefore largely fall onto **future development to freshly define the architectural appearance of the District.** While the design guidelines are not intended to create the basis for particular architectural style, the process of defining the Palma District will be aided by the



Figures 5.4 and 5.5: Simulations of development options at the Freitas Site in the Palma District

design guidelines and land use concepts contained in this document. The desired characteristics for development in this district are illustrated in two photo simulations of different development options on the key Freitas site (Figures 5.4 and 5.5). The guidelines generally require the design of buildings that relate well to the street, afford human scale, and, in the case of some larger parcels within this district, integrate pedestrian plazas into the proposed development. These qualities will help create a viable, vibrant core for the East 14th Street South Area.

International and Cultural District

Due to the limited number and the limited size of available opportunity sites, as well as the high number of small properties, **future change in this District, more than in any other, depends on façade improvements and building remodeling.** It is therefore important that City-sponsored programs support this activity and that the Design Guidelines be used to generate remodeling results that are coherent and consistent with the district character desired for the

District. This includes the further development and enhancement of existing characteristics such as the number and diversity of small-scale businesses with international offerings, the strengthening of an attractive pedestrian-oriented public realm, and flexibility in terms of signage requirements to allow vibrant signage that does not take away from the pedestrian character of the District.

Gateway District

The larger scale of properties in this district and its location just to the north of the regionally important shopping mall Bayfair Center, at the intersection of East 14th Street and 150th Avenue make it likely that regional and community-serving uses will continue to locate in this area (see District Land Use Matrix in Section 4.1). On properties that do not abut residential uses, **practical building heights may exceed those possible in many other parts of the corridor**, a feature that would be consistent with the district location at the entry to South San Leandro. However, in the long-term it is desirable that this district develop a more pedestrian-friendly relationship to the street as much as this is desired for all other districts of the corridor. **Buildings should be built at the property line and parking areas be moved behind the building, into internal parking courts or structures.** In addition, larger buildings built in conformance with the design guidelines would display a larger degree of orientation of building entries, façade and overall design toward the needs of the pedestrian and the human scale in general.

5.2 Design Guidelines

5.2.1 Site Development

These guidelines address how a given site and building should be accessed from East 14th Street, or how alternate means of access should be provided and designed.

Site and Building Access

- a) All street-level uses should provide primary pedestrian access directly to the street they front upon. Secondary access may face rear parking lots and other interior block spaces, such as pedestrian corridors.
- b) The primary façade of a building that orients to a public street or open space should contain the primary entrance(s) (Figure 5.6).
- c) Where feasible, a small hardscaped entry area or plaza should be located between a primary office or residential lobby and the sidewalk or pedestrian way. These areas



Figure 5.6: Clearly defined primary entrance

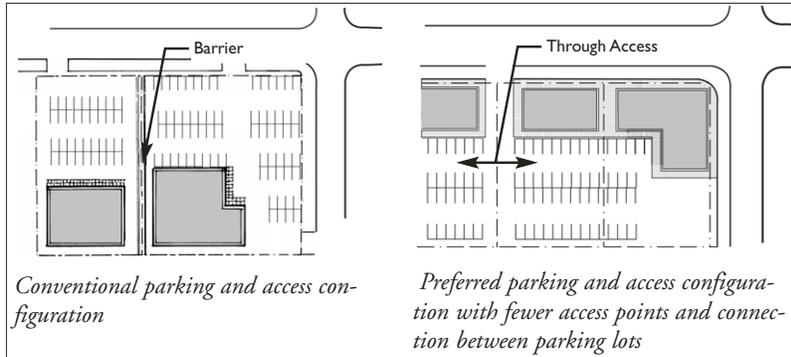


Figure 5.7: Off Street Parking Layouts.

should include simple amenities such as benches, planters with seating walls, deciduous shade trees, trash receptacles, and ashcans.

- d) When possible, no more than one curb cut (including both ingress and egress) should be provided per lot (or parking lot) onto East 14th Street in order to minimize conflicts with pedestrians, on-street parking, and street tree planting along sidewalks. Vehicular access to parking and services should occur from side streets rather than from East 14th Street, whenever feasible.
- e) Where possible, driveways should be located a minimum of 50 feet away from any intersection.
- f) Access between off-street parking lots on adjacent properties is strongly encouraged in order to reduce the number of curb cuts on East 14th (Figure 5.7).
- g) Where feasible, adjoining properties should share driveway access to off-street parking and service areas in order to reduce impacts to pedestrians and streetscapes.
- h) The widths of driveways should be minimized to 20 feet in order to reduce their presence along streets.
- i) To minimize their visual impact, entries to parking garages or driveways should not be across from side streets that terminate on East 14th Street T-intersections (Figure 5.8 illustrates what conditions to avoid).
- j) Service access from rear alleys or side streets should be preserved and enhanced wherever possible. Trash and loading areas should not be visible from the major thoroughfares and should be screened from view from side streets and adjacent properties to the rear. (See also: Building Service Elements, and Parking and Screening)



Figure 5.8: Parking garage entries should not align with side streets terminating on East 14th Street



Figure 5.9: Simplified building massing reduces pedestrian interest and degrades the visual environment



Figure 5.10: Overly articulated massing can be confusing



Figure 5.11: A building form with well defined "base" and "top"



Figure 5.12: Example of massing that reflects typical parcel sizes

- k) Secondary Frontage transition to residential uses should incorporate changes in frontage setback, height and landscaped buffers (see Section 5.2.4) as indicated in the City of San Leandro Zoning Code and in these guidelines.

Building Massing (Height and Setbacks)

The combination of height and setbacks of a building (also referred to as 'massing') defines the spatial relationship between a building and its uses as well as the public realm of the street and adjoining uses. The guidelines in this section discuss the desired setbacks, building envelope, heights and frontage for the following categories.

General

- a) Buildings should not be reduced to conveying building massing as a primary architectural gesture. Simplifying buildings to this extent creates monotony and a lack of detail, which is not conducive to creating an interesting pedestrian environment. (Figure 5.9)
- b) Building form should follow a logical order and rhythm. Overly articulated mass that is too complex can be chaotic and confusing, particularly in mixed-use areas. (Figure 5.10).
- c) In general, building form should provide a "base" and a "top" that are human-scaled both in terms of form, particularly in height, and articulation. (Figure 5.11)
- d) A well-defined "base" should consist of, but not be limited to thicker walls; richly textured materials (i.e. tile or masonry treatments); special materials such as ceramic tile, granite and marble; darker colored materials; and/or panels.
- e) Buildings with a frontage greater than 40 feet should make use of bays, recesses, overhangs, and other massing elements to reduce the scale of the building to the pedestrian level.
- f) The design of building massing should reflect and make visible the use and activity within the building. For example, the use of bays should reflect an interior change of use or function, such as a dining room or a private office.
- g) For multi-story buildings, the ground floor should be proportionally higher and architecturally distinguished from the upper façade to afford generous and inviting commercial spaces and to distinguish uses in mixed-use buildings.

- h) The rhythm of architectural massing should be compatible with the spacing of desired storefront or residential patterns within a given district. This type of massing should be combined with variations of vertical and horizontal façade articulation, roof shapes, architectural detailing, fenestration, and materials to provide interest (Figure 5.12).
- i) While the overall massing of buildings may be horizontal, elements of building form or detail should provide vertical articulation to avoid an overly horizontal orientation in buildings (Figure 5.13).
- j) Major features such as stairs, elevators, and major entrances should be expressed with vertical elements to avoid an overly horizontal look. Entry locations within the street frontage of a building may be given architectural emphasis through variations in building height or roofline. These features may also be used to create landmarks (Figure 5.14).
- k) Buildings on corner lots should give architectural emphasis to the building corner by incorporating a tower, plaza recess, or other building or urban design elements. Any such elements should be well-proportioned in relation to the average height of the building (Figure 5.15).
- l) The height and massing of those portions of corner buildings, which front onto streets intersecting with East 14th Street, should step down and set back to create a harmonious transition to existing adjacent buildings or buildings allowed under existing zoning



Figure 5.13: Vertical elements in the building facade helps avoid an overly horizontal orientation



Figure 5.14: The distinct vertical element of the main entrance helps in creating a landmark



Figure 5.15: Architectural Emphasis on building corners should be in proportion of the overall built form

Height and Setbacks

- a) Buildings fronting onto East 14th Street, between Maud Avenue and Blossom Way should maintain a continuous, minimum 4.5-foot front setback at ground floor level along the street to provide a wider (13.5-foot) sidewalk. The 4.5-foot setback may be increased to provide recessed storefront entrances, a special corner feature, or usable open space, such as a pedestrian plaza, outdoor dining or a small residential ‘front-yard’ (Figures 5.16 and 5.17). Additional guidelines for design of these areas are included in Section 5.2.5, Open Space and Landscaping.
- b) Buildings with ground floor residential or live/work uses may be set back from the front property line up to 10 feet, if this space is used to accommodate landscaping that both



Figure 5.16: Setbacks can create an outdoor dining space opening and widen the usable sidewalk



Figure 5.17: A residential building with suitable setbacks from the sidewalk



Figure 5.18: Setbacks on residential properties can be landscaped to enhance the visual experience on the sidewalk

enhances the public realm and the sense of privacy for residential units on the first floor (see example in Figures 5.17 and 5.18).

- c) Non-residential buildings or projects along East 14th Street should maintain zero setbacks for interior side yards, as outlined in the proposed Zoning Code amendments. This will help to maintain and establish a continuity of the street façade.

- d) Building height impacts should be carefully evaluated for sites adjoining residential districts, and should refer to the City of San Leandro Zoning Code for setbacks for each floor. Use of solar access studies can further suggest preferable setbacks for each floor, and can identify mitigation measures that may be needed to address impacts on adjacent residential lots (Figure 5.19). Buildings should be oriented to take advantage of the sun or outdoor private/public open space areas. Year-round solar access for adjacent buildings is encouraged. (Figure 5.20).

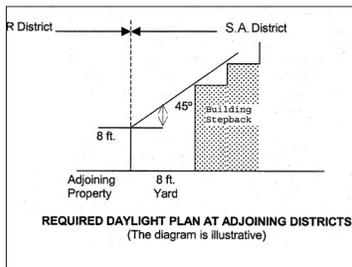


Figure 5.19: City of San Leandro Daylight Requirement Diagram

- e) On properties with buildings that abut existing ones, landscaped screening should be provided along the abutting property line in an area of 8 feet minimum width, to facilitate a visual transition between buildings of different scale.

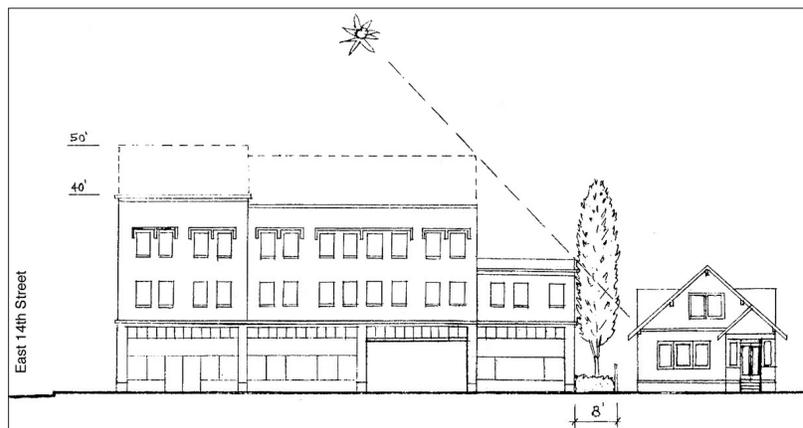


Figure 5.20: City of San Leandro Solar Access Diagram

5.2.2 Building Design Standards

This section addresses building and façade articulation, placement of doors and windows, façade details (i.e. bays, arcades and awnings) and building materials, such as colors and surface treatments. The guidelines are intended to promote façades with an overall rhythm and structure, spacing of entries and treatment of façade elements at the street level that are attuned to pedestrian scale and the pedestrian's perception of the environment.

General

- a) Buildings should take into account the urban environment and should not stand out as landmarks if they are part of the overall fabric. Rather, landmarks should be reserved for significant community buildings.
- b) Buildings should create a well-defined building wall. In general, building walls should be parallel to sidewalks to avoid creating ambiguous spaces that are devoid of human activity and which can become neglected over time.
- c) Buildings should form a continuous building wall that maximize building frontages onto streets with few interruptions from parking lots, driveways and inactive open spaces. The desired minimum frontage should be roughly more than three quarters of the lot frontage at the ground floor level.
- d) In no case should any façade consist of unarticulated blank walls as they do not contribute to the pedestrian realm and diminish a pedestrian's sense of security.
- e) Façade elements (i.e. windows, doors, bays, joints, etc.) should display a logical rhythm and order. To the degree that it provides interest to the pedestrian, articulation should be simple in form, because an overly articulated and random environment can be visually confusing and fragmented.
- f) Articulation and detailing should not consist solely of color changes without changes in material or planes, as color change alone does not create a feeling of permanence or real variety and interest.
- g) "False" fronts should not be allowed (i.e. all visible sides of a building should have a consistent style and use of materials). The primary exterior finish, for example wood or stucco, must be used on all façades of a unit or building visible from a street, pedestrian corridor, park, plaza, or other public or semi-public space.

- h) Mirrored or smoked glass should not be used as they eliminate the desired transparency of windows, cutting off the visual connection between pedestrians and activity within a building.¹

Transitions to Side Streets

- a) On corner lots, guidelines for Primary frontage should apply to side street frontage as designated in Figure 5-1.
- b) Secondary frontage transition to residential uses should incorporate changes in frontage setback, height and landscaped buffers (see 5.2.4) as indicated in the City of San Leandro Zoning Code and in these guidelines.

Ground Floor Storefront Retail or Office

- a) Primary building façades should be oriented to East 14th Street.
- b) Storefronts should be built up to, and parallel with, the front setback line, and should align with the adjacent building façades wherever they exist.
- c) Sidewalks shall be fronted by the more active uses of a building (i.e.; retail shop fronts, office windows, lobbies, building entrances, residential stoops, porches, yards, or courts, etc.).
- d) Where uses such as offices and banks occupy the ground floor of a building, the need for privacy and security should be balanced with the need to create a pedestrian-friendly street frontage.
- e) Large blank wall surfaces (more than 12 feet in length) should be articulated with vertical elements or material detailing, or should be interrupted by a window or entry. Artwork and landscaping are encouraged.
- f) Transparent, full-size windows should dominate the ground floor building frontage (Figure 5.21 and 5.22).
- g) Careful consideration should be made of window and door signage (temporary or permanent), shading and screening devices, and interior displays such that transparency is not significantly diminished by these elements.



Figure 5.21 and Figure 5.22: Desirable clear windows should dominate at the street level



Figure 5.23: Example of restaurant with window walls that open on to the sidewalk

¹The use of other glass products, such as Special ‘E’ films, can maintain transparency while providing solar protection and heat reduction for building interiors.

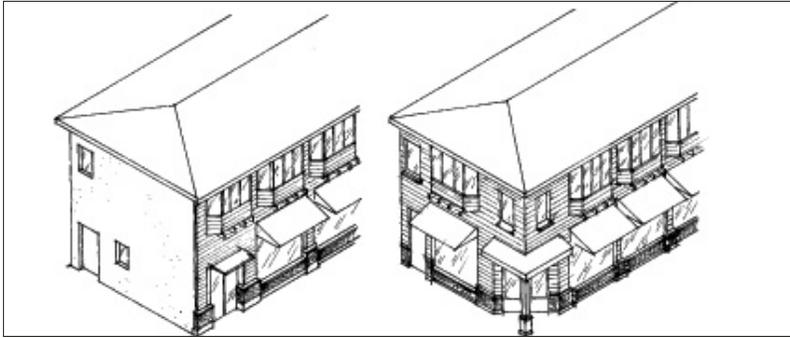


Figure 5.24: The continuity of façade materials at exposed building corner is essential to the side street.

- h) For restaurants and cafés, consideration should be given to providing window walls that can be opened to the street to provide indoor/outdoor-dining opportunities (Figure 5.23).
- i) Corner buildings should continue the window pattern of the primary East 14th frontage for the length of the Primary frontage designation and at a minimum of one bay along the intersecting street (Figures 5.23 through 5.25).
- j) Window design should maximize interior daylighting while reducing glare through the use of passive shading devices that maintain visibility between the exterior and interior of the building. Methods can include: "special E" glass technologies, deep overhangs or external sun shades, and trees
- k) It is strongly encouraged that entries to retail spaces, restaurants and cafes be recessed to increase circulation space available to pedestrians who enter and exit businesses (Figure 5.26).



Figure 5.25: Example of facade treatment that provides continuity in materials and appropriate scale transition from primary to secondary frontage

Upper-Floor Commercial Facades

- a) Facades near residential uses should use window placements and designs that restrict views from within the structure into nearby yards and homes.

Ground-Floor Residential

- a) Frontage onto streets should include a transition zone from public to private, rather than an abrupt separation. Porches, large windows, stairs, and welcoming entryways provide opportunities for a friendly transition between public and private spaces.
- b) Side elevations facing public and private streets and pedestrian corridors should be treated in a similar manner as the primary front façade and may include wrap-around porches, bay windows, dormer windows, and balconies.



Figure 5.26: Recessed entries in restaurants and cafes increase circulation space for patrons

- c) Stoops, balconies, verandas, and bay windows should be encouraged and allowed to encroach into the front yard set-back or public right-of-way.
- d) Where street-facing, first-floor residential units are set back up to 10 feet from a public right-of-way, these should be designed with additional measures to ensure privacy. At a minimum, windowsill heights should be raised above the eye level of a passing pedestrian. Elevated stoops and raising interior floor elevations above adjacent sidewalk grade are some measures that can be employed.
- e) Façades set back more than 6 feet should be designed with additional measures to create a pedestrian-friendly frontage. This could include low walls and fences (no higher than 4 feet); entry features such as arbors, gates, courtyards, and walkways; lighting; and landscaping.
- f) For courtyard configurations, facades which front onto a public sidewalk should be treated as a primary façade with front entrances directly facing sidewalks and appropriate detailing and fenestration.

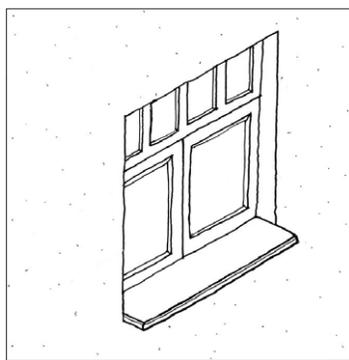
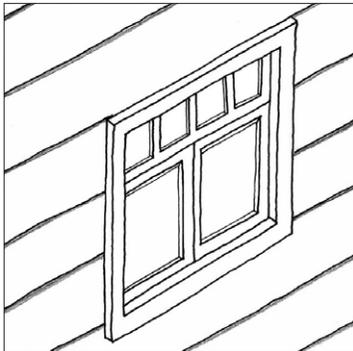


Figure 5.27 and 5.28: Window recesses cast complex shadow lines, creating interest that attracts the eye of pedestrians

- g) In multi-family developments, rental offices, community rooms, exercise rooms and other appropriate ‘community’ spaces should front onto main thoroughfares to increase the connection between uses in the building and the street.
- h) Sidewalks, pedestrian corridors, and other open spaces should be fronted by the more active uses of a building. For residential uses, this includes lobbies, porches to individual unit entries, living rooms, and dining rooms.
- j) Façades that front onto public streets and pedestrian corridors should be lined with windows. Windows facing the street should be at eye level and give a sense of habitation and security. They should encompass approximately 50% of the façade length fronting onto the street. Side facades on corner units facing pedestrian routes should be treated in a similar manner as the main façade.
- k) Window and window frame should be set to provide a reveal, trim and recess (not be flush with the exterior face of wall) to provide a shadow line (examples are shown in Figures 5.27 and 5.28). Window glazing should be clear; reflective or tinted glazing are discouraged.

- l) For privacy reasons, in cases of units with narrow side yards, side elevation windows should be placed so as to be off-set from those of an adjacent unit facing the window.
- m) Residential lobby entries in mixed-use and residential buildings should be clearly defined, distinct from commercial entries, and be directly accessible from the main thoroughfare (Figure 5.29).
- n) In multi- and single-family buildings, ground floor residential entries should be oriented towards and be directly accessible from the street.
- o) Ground floor residential units are encouraged to provide an entry porch with minimum clear dimensions of 8-foot wide by 6-foot deep; this provides a transition from the public realm of the street to the private unit while providing residents the opportunity to occupy the porch and interact with the surrounding community (Figure 5.30).
- p) Porches should be designed as an integral architectural feature of the main structure. Porches should be covered with a roof that is supported by posts. Posts and rails should be substantial in appearance.



Figure 5.29: Clearly defined residential entry in a mixed use building

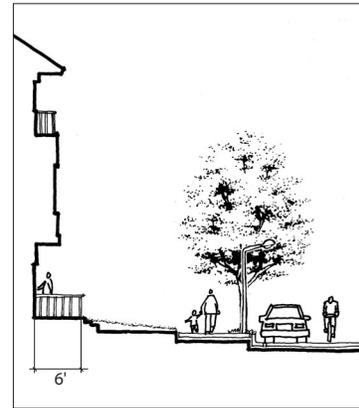


Figure 5.30: Entry porches for residences provide a transition from the public realm of the street to the private unit

Upper-Floor Residential

- a) Bays and balconies should allow for a minimum 10-foot clear height above any sidewalk, unless set back from the build-to line.
- b) Balconies should be a minimum of 6 feet in depth. Juliet balconies (i.e.; balconies with a flush or minimal depth) are also allowed and should have a minimum 8-foot-wide door opening connecting the balcony with the interior. This allows the balcony to function as an extension of the room. (Figure 5.31)
- d) For corner units, balconies may wrap around the unit.
- e) Guidelines for upper-floor windows should follow those outlined within the Ground Floor subsection.

Automobile Services (Remodel)

- a) Remodeled or improved façades abutting public sidewalks should incorporate a level of articulation and detailing similar to that of commercial retail frontages along the corridor.



Figure 5.31: Juliet balconies help create a human-scaled connection between upper floors and the street.



Figure 5.32: Auto Service facade with work area and no setback lends interest to passerby



Figure 5.33: Auto service facade with clear windows are compatible with retail frontages



Figure 5.34: Example of blank walls and roll-up door degrades the pedestrian environment

- b) Remodeled or improved façades of auto service and repair businesses should incorporate clear windows, and an entry to the supporting office use, along East 14th to create a visual relationship between public sidewalk and indoor activities (Figure 5.32 and 5.33).
- c) Blank walls and opaque door facades are not acceptable (Figure 5.34).

Building Entrances

- a) A clear distinction should be made between primary entrances and secondary entrances. Primary entrances should be designed to include greater detail and ornamentation to give them a clear identity and separate them from more minor entrances. The building address should also be signed at the primary entrance.
- b) Primary entries should be clearly expressed and recessed or framed by sheltering elements such as awnings, arcades, porches, or porticos. This allows protected space for people to meet or pause as they enter or leave the building. Secondary entries should be treated in a similar, but less prominent manner.
- c) Primary entrances are encouraged at street corners. Orienting primary entrances to street corners, creates definition at intersections, which are important meeting points and are prominent places of identification. See also Building Orientation for corner treatment.

Awnings and Canopies

- a) In general, the use of awnings should be reserved for commercial ground floor uses. Windows above the first floor should be treated with other types of shading devices. Preferred types of shading devices could include the examples seen in Figures 5.35 and 5.36.
- b) Awnings and canopies should be proportional to the facade on which it is placed on a building and not obscure architectural elements and details. Awnings and canopies should not be dominant or overwhelming elements
- c) Awnings should reflect facade articulation (Figure 5.37).
- d) Awning and canopy heights should provide a pedestrian

scale to the building and be compatible with other awnings nearby, particularly those on the same building, if such existing awnings complement the architectural character of the building.

- e) Color-fast canvas fabric for awnings are preferred over vinyl, fiberglass, plastic, wood or similar materials. Glass and metal canopies may be appropriate for some buildings, but must be consistent with the architectural style of the building. Internally illuminated awnings are discouraged.

Roof Design

- a) Roofs should not be cosmetic in character or appear to be ‘tacked on’, but rather be an integral part of the overall building design relating to entries and overall façade articulation and building massing. In general, the design and detailing of roofs should provide a sense of termination to the building façade with an overhang or other design element.
- b) The profile created by roof forms should be simple, with no unnecessary changes in plane; roof configuration should reflect a building’s floor plan and massing.
- c) Roofs may be flat or sloping. Mansard roofs are discouraged.
- d) Flat roofs should be designed with an articulated parapet.
- e) Incorporate the location, spacing and screening of rooftop mechanical equipment into the general roof and building design. (Figure 5.38)
- f) The roofs of buildings on corner lots should give emphasis to the building corner.
- g) Main rooflines and roof detailing should consider the context of adjacent buildings and existing roofs nearby along East 14th Street.

Exterior Lighting

- a) Exterior lighting should be shielded, directed downward, and on-site. It should not produce glare that negatively affects pedestrians, bicyclists, motorists, or adjacent uses. It should be consistent with the City of San Leandro Zoning Code requirements related to average candle foot output.
- b) All exterior lighting should be integral part of a building’s architectural design and scale.



Figure 5.35: Example of upper-floor window treatments shading divides that are compatible with, yet distinct from those of the first floor level



Figure 5.36: Shading devices can help to create interest in a building’s facade

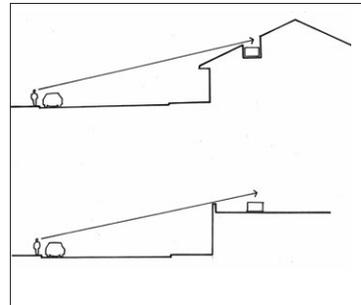


Figure 5.38: Proper screening of rooftop mechanical equipment



Figure 5.37: Example of canopies that are framed in the storefront without blocking clerestory windows

- c) Where appropriate, consider accent lighting to highlight interesting architectural features, signs, and storefront displays.
- d) Use of neon lighting for signage and architectural highlighting is acceptable in the Palma, International and Cultural, and Gateway Districts, if it is in keeping with the character of existing neon signage in these districts.

Building Materials, Colors, and Paint

- a) To give buildings an authentic appearance, as opposed to a veneer-like quality, material changes should not occur at external corners. Rather, they should occur at interior corners or at a change in horizontal plane.
- b) The amount of reflective building materials should be limited on development directly abutting a pedestrian way. Highly reflective material, such as mirrored window walls, on building facades may help to keep interior temperatures down, but can be uncomfortable for the pedestrian passing by. Other affordable and effective means of energy efficient design are readily available, such as “special E” films.
- c) A variety of building finishes and materials are appropriate for pedestrian-supportive architecture, such as wood board siding, wood shingles, tile, stucco, masonry, and higher quality curtain-wall systems that provide shadow lines and scale through the use of mullions that provide relief. Materials that do not age well should not be used, such as scored plywood, (i.e. T-111). If other ‘simulated’ materials are used, they should be of a quality, color, and application that demonstrates a convincing realism.
- d) Building colors should be tastefully coordinated and, where appropriate, match or complement those of adjacent buildings, subject to design review.
- e) Building surfaces and other finishes should be selected to ease graffiti removal. Consider the use of graffiti removal paint or coating at locations that may be prone to graffiti.

Signage

Signs are an extremely visible part of the streetscape, and should reflect the quality of goods and services being offered on East 14th Street. They should communicate an image of excellence, distinctive craftsmanship, and creativity, and should reinforce the desired character of the East 14th Street.

In addition to the following guidelines, all signs must conform to the City of San Leandro Zoning Code (Article 18). Design approval and a building permit must be obtained prior to sign installation.

- a) Signage should follow a hierarchy that clearly indicates the importance of the associated use, building, or place.
- b) Signs should address primarily pedestrians, but also be legible for drivers. Addresses should be clearly visible for police and fire emergency response teams.
- c) Signs should reflect the character of the building and its use. It is imperative that the sign design add interest and beauty to the facade.
- d) Signage should respect the immediate context of the building's location, and the character of the district along East 14th Street.
- e) The architecture of the building often identifies specific locations for signs, and these locations should be used.
- f) Signs should be an integral part of the design of storefront alterations and new construction. Signs should not obscure architectural elements such as transom windows or columns, nor appear cluttered.
- g) Signs constructed with high-quality materials such as metal, stone, wood, gold leaf or exposed neon are preferred.
- h) Coordinate the design and alignment of signs on multiple use buildings in order to achieve a unified appearance rather than visual confusion.
- i) Pedestrian-oriented signage should demonstrate a high level of detailing and craftsmanship.
- j) Flexibility should be granted to artisans and craftspeople whom may wish to create unique signage that may contribute to the sense of place.
- k) Established historic signs should be preserved, if feasible. Modify historic signs for new use such that the changes are compatible with the original sign. If possible, modifications should be removable without affecting original elements, to permit future restoration. The use of similar sign designs in



Figures 5.39 through 5.41: Examples of landmark signage along the East 14th Corridor

the Palma and Gateway Districts is encouraged (Figure 5.39 through 5.41).

- l) For lit signs, use of spot lighting, halo lighting, or exposed neon is preferred. Spot lighting should be inconspicuous or an integral design feature of the sign, and should not cause glare for pedestrians or motorists. Blinking or flashing sign lighting is prohibited by the San Leandro Zoning Code.
- m) Careful consideration should be made of window and door signage, shading and screening devices, and interior displays such that transparency is not significantly diminished by these elements.

Building Service Elements

- a) Secondary building elements such as garbage receptacles, utility meters and mechanical equipment should be accommodated within the building envelope whenever possible. If secondary building elements must be outside of the building envelope, they should be screened from view of pedestrians. Screening should be harmonious with the building design. See also section on Roof Design.
- b) Whenever feasible, utility meters in front of buildings should be installed below ground either on private property or in public sidewalks.
- c) Loading areas should not occupy more than 20 feet of building frontage.
- d) Solid walls should be landscaped to soften their appearance and should be made of finished materials to be compatible with the primary building. Decorative elements, variation in materials, and articulation should be used.
- e) On new construction, building utilities (i.e. plumbing and heating vents, etc.) should be grouped to minimize their visual impact on the roofs of buildings wherever possible.

5.2.3 Parking and Screening

The East 14th Street, South Area is home to many auto-oriented businesses, including retail, automobile services and auto sales. These uses are often associated with paved service yards, customer parking, or display areas, many of which directly abut public sidewalks. Screening of these parking areas and yards, however, is pro-

vided in only a few cases or in an insufficient manner. This condition negatively impacts the public realm in two ways: 1) unsightly yards and parked cars are exposed to view from public spaces, particularly sidewalks, and 2) spatial definition and continuity of the street edge is disrupted due to the lack of vertical elements along the property line, such as buildings façades. The latter is a particular issue at street intersections, where buildings should define the pedestrian space at street corners and create a connection to the adjacent neighborhoods.

It is therefore important that future development accommodates parking and yards in a way that mitigates the negative impacts of such uses on public areas and neighboring uses to the largest extent possible. Existing parking areas and yards should be retrofitted following the guidelines below to create greater spatial continuity and improvement of the pedestrian realm along East 14th, South Area. Retrofitting should be a condition of use permit approval or site plan review for auto-oriented uses.

General

- a) New parking areas should be accommodated behind buildings as surface parking or in a parking structure, in a central court, or in an underground facility (Figures 5.42 and 5.43). All surface parking should be buffered from public view.
- b) To the greatest degree possible, the exposure of structured parking should be minimized along streets and sidewalks, parks, and plazas. Structured parking should be buffered from public view with landscaping or should be "wrapped" around with ground floor building space.
- c) Garage structures should adhere to the same requirements as commercial buildings in terms of orientation, entries, design, and architectural elements. Upper floors of garages should follow the same guidelines for the design of openings as ground floors.
- d) Openings, other than auto entries, into parking structures should be designed as typical window and door openings. Although these openings will typically not include glass, they should be designed with elements providing similar articulation and detail to window sills, jambs, and headers.
- e) Blank, monotonous façades should be avoided both on the ground floor as well as upper stories of parking garages. To lend interest to facades, architectural details similar to those used for other commercial and residential uses should be uti-



Figures 5.42 and 5.43: Example of a residential development with courtyard parking



Figure 5.44 : Parking lot with trees and landscaping



Figure 5.45: Permeable paving helps reduce run-off water



Figure 5.46: Protected paths for people to walk across in parking lots

lized. This can include trellises, awnings, arbors, balconies, railings, public art, overhangs, and architectural façade details. Elevators and stairs should activate and articulate the façade of parking garages

- f) Landscape elements along pedestrian pathways should be utilized to reduce the perceived size of the lot and create a more pleasant microclimate for pedestrians.
- g) Deciduous trees should be used within surface parking areas to provide shading to reduce heat buildup and to improve visual appearance (Figure 5.44). The spacing of trees should consider the species and their growing habits.
- h) To effectively achieve this coverage, trees should be planted "orchard style" (i.e. evenly spaced throughout the parking lot).
- i) Patterned pavement surfaces are encouraged for primary driving entrances and key on-site intersections to provide visual interest. Consider incorporating 'Best Management Practices' (BMPs) for stormwater run-off into the design of parking lots such as permeable paving or landscape swales (figure 5-45)
- j) Parking areas should be properly lighted and have designated pedestrian paths to create a safe environment for persons going to and coming from their cars (Figure 5.46). Direct glare of lighting on adjacent residential properties should be avoided through proper location and shielding of light sources.
- k) Pavement markings and signage should be designed to enhance the safety and circulation within parking lots for all modes.
- l) Clearly delineated walkways, separated from traffic lanes, should be provided from parking areas to the entrances of establishments. (Figure 5.47)
- m) Walkways should provide a minimum clearance between car fenders of 5 feet.
- n) Where the path crosses the auto lane, the path should be clearly delineated by a contrasting color, pavement pattern, and/or be raised slightly to form a speed table.
- o) Trees should be planted along interior pedestrian paths to provide needed shade.

Tandem Parking

Tandem parking is a configuration of parking by which two stalls are accessed from the same parking aisle or driveway (Figure 5.46).

Tandem parking is more efficient in terms of land area and cost for parking, because the same aisle provides access to two rather than one space. But tandem parking requires that the last car into a set of tandem spaces must be moved before the first car can be moved; this requires coordination between the drivers of the two cars.

- a) Tandem parking is allowed to meet the parking requirements for single-family attached residences, multi-family residences where the two spaces are owned or rented by the occupants of a single unit, employee parking for commercial and office uses, and in commercial parking lots with valet parking.
- b) Tandem parking spaces as allowed above should be counted towards the parking requirement for a land use within the South Area planning area.

Provisions for Car Sharing

Car sharing is the practice of organized sharing of cars by individuals and/or businesses that want to reduce the cost of operating a vehicle by sharing it with other users. City Carshare is a non-profit organization in the Bay Area that supports the sharing of autos. The East 14th Street South Area Development Strategy supports car sharing by providing for a reduction in parking requirements for certain uses in the planning area if a car share vehicle is provided.

Screening of Parking Lots & Outdoor Use Areas

- a) Parking lot screening and landscaping along the sidewalk edge is required to provide a buffer for pedestrians walking along adjacent streets, to provide a shaded and comfortable microclimate, and to reduce the sense of auto-dependency and encourage the sense of ‘pedestrian equivalence’.
- b) Where adjacent to public sidewalks, parking areas (and gas stations) should include walls, trellises or arbors, artwork, or shrubbery between the sidewalk and the first row of parked cars (Figures 5.48 and 5.49). A maximum of 3’-6” tall, visually ‘solid’ buffer, such as a wall or hedge, should be provided to screen the parked cars. Walls, fences and trellises should be built with attractive and durable materials. (Figure 5.50)
- c) Similarly, commercial storage, outdoor display and sales areas abutting street frontages and residential should be visually



Figure 5.47: Example of clearly delineated path in parking lots



Figures 5.48 and 5.49: Examples of landscaped screen parking/gas station and between public sidewalks



Figure 5.50: Well articulated trellis screen between a parking lot and a public sidewalk

screened up to six feet with landscaping or the methods and materials mentioned above. If a wall is used, it should be combined with a landscaped planting strip.

- d) Wherever feasible, trees should be planted for screening in combination with measures described in section 5.2.1. Trees planted for screening may not cast a shadow onto preexisting solar panels on neighboring properties; stipulations of the California Solar Rights Act (Section 714 of the Civil Code) apply.
- e) The use of cyclone/chain link-type fences along street frontages or abutting residential properties is not allowed.
- f) Where permanent fences are needed, high quality fencing material should be used (Figure 5.51 shows a positive example). The possibility of including artwork in the design of the screen should be given strong consideration.



Figure 5.51: High quality permanent fencing.

Bicycle Parking

- a) Bicycle parking should be conveniently located to encourage their use as an alternate mode of transportation.
- b) Large projects should include bicycle racks within the project site at a location that is clearly visible from the public street or from active building spaces. When possible, locked and covered structures for bicycles should be provided and designed to be compatible with the architecture of the building.

5.2.4 Open Space and Landscaping

With only very few public open spaces in proximity to East 14th Street, it is pertinent to maximize the combined effect of landscaping associated with private development and public improvements. The guidelines that follow aim to increase the amount and quality of open space and landscaping along the corridor. Larger projects are encouraged to incorporate a pedestrian plaza that is accessible to project residents and the public. In addition, the guidelines promote landscaping to screen taller building volumes in new projects, to help create a smoother transition between structures of different scale and use.

General

- a) In large projects where publicly accessible pedestrian plazas are provided, the character of such plazas should be appropriate for the predominant use or mix of uses in a project.

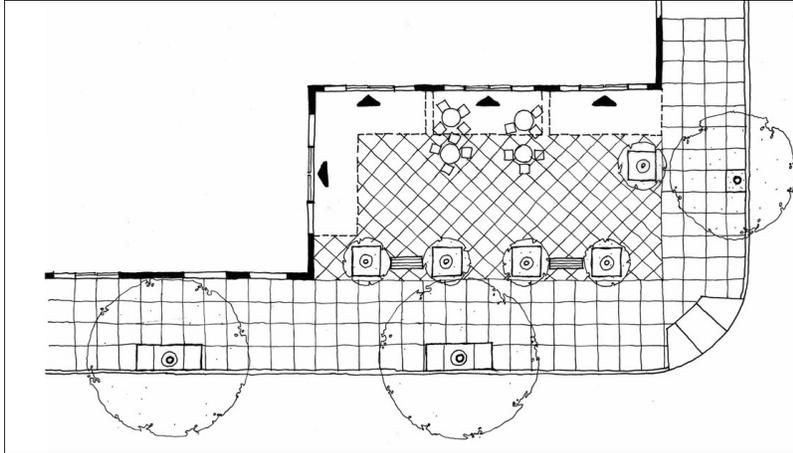


Figure 5.52: Corner plazas should be incorporated with quality landscaping

This may be an entry plaza, a plaza with seating for a cafe or restaurant, or a plaza with public outdoor seating and other amenities. All plazas should incorporate high quality landscaping that is an integral part of the overall building design and style. All plant material should be appropriate for the climate conditions at the plaza location (Figure 5.52 and 53).

- b) Mixed-use, residential and live/work projects should incorporate interior courtyards and/or rooftop gardens. The incorporation of semi-public open spaces is strongly encouraged (Figure 5.54).
- c) Along the frontage of a development project facing East 14th Street, private landscaping in planters, and the integration of outdoor seating into the building and site design are strongly encouraged (Figures 5.55).
- d) Existing specimen and heritage trees on private properties should be retained and properly protected during construction.
- e) Landscape design, plant selection, and maintenance should provide for safe visibility between vehicles, pedestrians, and bicyclists.

Landscaping for Buffers and in Setbacks

- a) On properties with new buildings of three or four stories and abutting existing residential uses or properties zoned for residential, screening should be provided along the property line adjoining the residential area within a planting strip of 8 foot minimum width. Such screen should consist of landscaping and a fence of six feet maximum height (Figures 5.56). The



Figure 5.53: Corner Plazas can significantly enliven the public realm



Figure 5.54: Interior courtyards in mixed used buildings



Figures 5.55: Integrated outdoor seating with public right-of-way

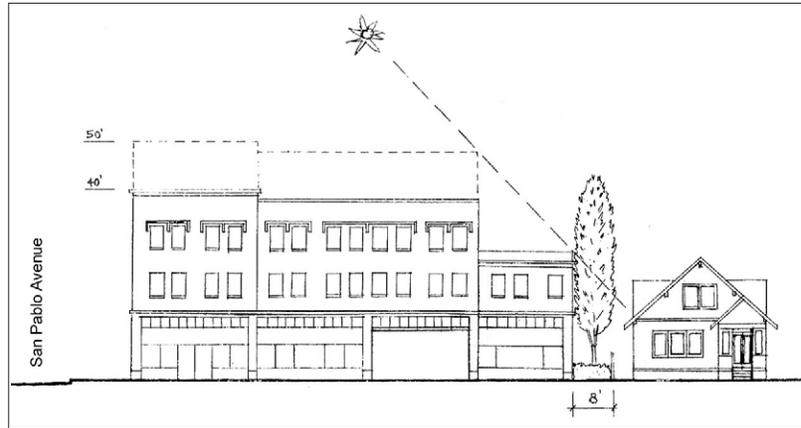


Figure 5.56: Adequate screening should be provided for single-story residential uses next to buildings more than 4 stories

landscaping should include columnar deciduous trees planted at 20 feet on-center. The provided fence should be suitable for residential settings.

- b) Trees planted in the 8-foot setback may not cast a shadow onto preexisting solar panels on neighboring properties; stipulations of the California Solar Rights Act (Section 714 of the Civil Code) apply.

Residential Yards

- a) Buildings with ground floor residential uses may set back from the front property line up to 10 feet, if this space is used to accommodate landscaping that both enhances the public realm and the sense of privacy for residential units on the first floor. The landscaping within the setback should be an integral part of the overall building design and style. Plant material should be high quality and appropriate for the climate conditions at the yard location.
- b) Front yards can be defined by a fence or hedge of no more than three feet in height. Any portion above 3 feet should be visually permeable.
- c) Entry gates can be enframed by a gateway. Both fences, and gateways should be built with attractive and durable materials. The use of cyclone/chain link-type fences is not allowed.

